

STAMFORD TOWN HIGHWAY ORDINANCE

SECTION 1.00 - INTRODUCTION

The Selectboard of the Town of Stamford, County of Bennington, State of Vermont, in accordance with the authority invested in them by virtue of Title 24, Section 1972 through 1976 of the Vermont Statutes Annotated, hereby adopt the following Stamford Town Highway Ordinance.

This ordinance is in furtherance and support of the proper accomplishment of responsibilities and the discharge of authority prescribed by 19 VSA Chapter 7, Laying Out, Discontinuing and Reclassifying Highways by Local Authorities.

This ordinance is designated as a civil ordinance pursuant to 24 V.S.A. 1971(b). A violation of this ordinance shall be a civil matter enforced in accordance with the provisions of 24 V.S.A. 1974 (a) and 1977 et seq.

1.01 DEFINITIONS

BANK: Mound of earth along a road.

CLEARING: The process of removing obstructions from the road site.

COLD MIX: Cold bituminous concrete.

CRIBBING: Any material used to keep dirt from caving in.

CROWN: The upper part of the rounding surface of a road.

CURVES: Turns in a road.

CULVERT: A pipe under a road to carry water.

DITCH: A long narrow trench to carry water away from the road.

DRAIN: A trench or ditch to carry water away from the road.

DRAINAGE RIGHTS: Town has rights to maintain drainage for the road.

DRIVEWAY: Any roadway permitting accessibility to private property from any Town highway by any vehicle including gasoline, diesel, propane, or horse drawn.

EMBANKMENT: A bank of earth used to hold water back or hold up a roadway.

EROSION: The act of wearing away the ground.

GRADE: To make the ground level or sloped evenly for a roadway.

GRADING: The act of bringing to grade.

GRAVELING: The act of laying gravel on a road.

GUARD RAIL: Posts with connecting rails to keep vehicles on the road.

HEADER: A pipe that connects other pieces to permit the flow of water through them.

HOT BITUMINOUS CONCRETE: Blacktop.

INFILTRATION: The process of water passing through small gaps.

MOTOR VEHICLE: All vehicles propelled by other than muscular power, regardless of where registered, except fire apparatus and emergency vehicles.

PAVING: The act of laying a hard surface on a road (i.e. oil and stone, cold mix, or hot bituminous concrete).

PENT ROAD: A shut in road, no access to it.

PLAN: An outline; a draft; a map.

PLAT: To make a drawing or plan of.

RIGHT-OF-WAY: A strip of land over which a public road passes.

RIPRAP: Broken stones placed to prevent bank erosion.

SELECTBOARD: Also means their authorized representative.

SHOULDERS: Either edge of a road or highway.

SLOPES: A piece of ground that is not flat or level; rising or falling ground.

SLOPE RIGHTS: Town has right to maintain slopes to protect road.

SUBBASE: Layer of material above subgrade.

SUBGRADE: Just below the ground level after clearing and grading the site.

STUMPING: Removing tree stumps.

TRAIL: A path or track made by continual passing or deliberately blazed.

TRAVELED WAY WIDTH: Actual road width.

TURN-A-ROUND: A circle at the end of a dead end road with a diameter of at least one hundred feet to allow vehicles to change direction.

UNDERDRAIN: Trench below the surface of the road so formed to admit water percolating through the gravel above.

UPPER BASE: Layer of material above subbase.

SECTION 2.00 - GENERAL CONSIDERATIONS

2.01 GENERAL

The Selectboard may, at their discretion, modify the requirements contained herein if their strict enforcement would cause undue hardship due to unusual conditions, provided the general objectives of these requirements are satisfied.

Compensation for damages to town highways caused by acts of noncompliance with this town highway ordinance may be assessed against violators by the Selectboard.

2.02 ROAD CLASSIFICATION

Town roads are classified as Class 1, Class 2, Class 3, or Class 4 highways in accordance with 19 V.S.A. Chapter 3 Section 302. An official town highway map supplied by the State Agency of Transportation shall be available for inspection in the Stamford Town Office.

2.03 ROAD RECLASSIFICATION

The Selectboard shall periodically review the town highway classifications, taking into consideration increasing road need, changing traffic patterns and growing population, including identification of key roads that link towns and major arteries. Selectboard may also discontinue certain roads, either classifying them as trails while retaining a right-of-way or abandoning any claim to a right-of-way.

2.04 ROAD CONSTRUCTION AND MAINTENANCE

Road and related improvements shall be constructed or installed in accordance with sound engineering practice and this town highway ordinance.

The Selectboard shall institute and periodically review and update a Town road management system in making decisions about preventative maintenance and road improvements in accordance with 19 V.S.A. Chapter 3, Sections 304 and 305. The plan shall include but not be

limited to considerations of scenic areas and places of local historical significance, signs, bridges, etc.

2.05 CLASS 4 ROADS

Class 4 roads, not including trails and pent roads, may or may not be maintained by the Town in summer months only at the discretion of the Selectboard. Such maintenance, if approved by the Selectboard, will be limited to preventing deterioration and may include replacing culverts, removing debris from the roadway, repairing washouts, placing gravel, grading, cutting brush, repairing bridges, and removing downed trees.

Selectboard shall exercise control of Class 4 roads to ensure their integrity as a public right-of-way by means which may include, but are not limited to the following:

- a. prohibition or restriction of wheeled vehicle use during muddy, snowy and other adverse conditions; signs and barriers may be utilized to accomplish this purpose;
- b. requirements for temporary permits for heavy equipment access or plowing may be imposed and the stipulation included that any highway damaged will be repaired by or at the expense of the user; posting of bond or other security to guarantee that repairs are made may be required as a condition of any permits.

All Class 4 roads will be closed from December 1st to April 30th. Accordingly Class 4 roads shall only be plowed with permission granted by the Selectboard.

2.06 RIGHT-OF-WAY ACCESS

Selectboard shall control access into the road right-of-way for the installation or repair of utilities and access of driveways, entrances and approaches.

2.07 OVERWEIGHT VEHICLES

Selectboard in accordance with 23 V.S.A. Chapter 13, shall issue permits for vehicles exceeding posted load limits.

In some cases, the Selectboard will negotiate with the applicant for compensation of estimated road damages. Only after both parties agree to the terms will a permit be issued. The Selectboard, or their agent, will inspect the proposed route both before and after the trucking operation. Probable damages to culverts, bridges, road surfaces and bases will be considered in the estimate.

Before issuing an overload permit, the Selectboard shall also consider weather conditions and seasons of the year.

All damages to the road, bridges, culverts and related structures caused by overload by applicant or by his negligence are the responsibility of the applicant and shall be repaired under the supervision of the Selectboard or their representative and at the expense of the applicant. Failure

to repair said damage will result in the Town completing the work and billing the applicant.

2.08 POSTING

No town road of any class may be intentionally closed by a gate or other obstruction by anyone other than the Selectboard.

Selectboard may post a road in accordance with 19 V.S.A. Section 1110. The Selectboard may post a highway for the purpose of preserving the integrity of the highway.

2.09 COMPLIANCE WITH OTHER REGULATIONS

The Town of Stamford has other ordinances, therefore this Highway ordinance by itself is not inclusive.

2.10 LAYING OF UTILITIES IN TOWN HIGHWAYS

The property owner who wishes to lay utilities under a town highway shall obtain a permit from the Road Commissioner of the Town of Stamford. Permit is valid for one (1) year but can be extended for one (1) year upon written request to the Road Commissioner.

Upon application for a permit to lay utilities under a town highway, the owner shall submit, in writing the plans, including but not limited to locations, all of which must meet the approval of the Selectboard's agent, the Road Commissioner.

Road surface will be cut to provide a straight edge.

All material in the traveled portion of the road to the full depth of the ditch (6 feet minimum) will be removed and replaced with 1 ½ inch crushed gravel or equivalent approved by the Selectboard or their representative. All other road crossings, i.e. borings, etc. will be a minimum of 6 feet unless otherwise approved by the Selectboard or their representative.

The utility to be installed will be sleeved in 4 inch schedule 40 minimum for the full width of the town highway.

Trench will be filled in 1 foot intervals and compacted by mechanical means, i.e. Wacker, Jumping-Jack, etc., in traveled portion of highway.

There will be a minimum of 4 inches of hot asphalt to complete trench in traveled portion of highway.

All other town property will be restored to its present condition, ditches, roadsides, etc. within seventy-two (72) hours from date of excavation.

Job will be properly signed and due to liability to the town, a representative of the town, either

the road commissioner, constable or duly appointed representative of the Selectboard, will be present for traffic control until the highway is restored. Payment for their time will be paid by the permit holder.

Permit holder will be responsible for road surface sagging and/or deterioration for one year from date trench is completed.

No work will start in town's right of way until permit holder agrees upon all conditions of permit and all permits and fees are in place. This permit is for one day. Ditch must be closed or plated at end of workday. If additional days are needed then a permit must be obtained for each additional day.

The town will be notified a minimum of three work days prior to date and time of proposed project so coordination may take place between proper authorities.

Upon completion of laying of utilities of which a permit was obtained, the Road Commissioner shall inspect the site. If it does not meet with the Road Commissioner's approval, the owner shall be deemed to be in violation of the Stamford Town Highway Ordinance.

It is the land owner's responsibility to maintain and repair any and all future damages caused by the laying of underground utilities in the Town Right of Way.

(See appendix 6.03 for application form.)

SECTION 3.00 - ROAD ACCEPTANCE POLICY

3.01 GENERAL

It is the policy of the Selectboard of the Town of Stamford to entertain application for the acceptance of public roads and to adopt and adhere to an orderly procedure for the receipt of and acting upon such applications. The decision on an application for acceptance of a road is reserved to the sole and absolute discretion of the Selectboard who will consider the grant or denial of an application on the basis of the best interest of the inhabitants of the Town. The Selectboard will elicit input from the townspeople through a warrant article at the annual town meeting. This vote of the townspeople will be advisory only.

An applicant shall secure all other necessary governmental permits for a land subdivision or for road construction as a condition precedent to the acceptance as a road.

All questions arising during road construction relative to construction methods, materials or specifications shall be answered by reference to this ordinance and to the Vermont Standard Specifications for Construction.

The Selectboard or their representative shall make final decisions over all questions arising during construction of new roads and shall approve all field changes.

Any new road, whether or not that road is proposed to be conveyed to the town shall be constructed according to the minimum standards and other terms of this ordinance.

3.02 APPLICATION, PLANS AND PLATS

A plan of proposed roads shall be submitted to the Town for review. The plan shall be in harmony with existing or proposed roads. All roads shall have free access to or shall be a continuation of one or more accepted Town roads or highways. The grade line and minimum radii of all curves must be shown.

A written application for the acceptance of a road shall be submitted to the Town six (6) months before the annual town meeting in March and before road construction is started. The application shall be accompanied by the following:

1. Three copies of a survey map of the existing road prepared by a registered surveyor based on the original recorded description and a survey based on the existing road bed,
2. Three copies of a plan of the proposed upgrade showing grade lines, plan and profile of the road and drainage layout (location and size),
3. A proposed warranty deed of the land to be conveyed for road purposes,
4. A list of the names of all property owners who abut this portion of the road purposes. The grade line and minimum radii of all curves must be shown.
5. A fee as described in Section 3.03.

All monuments shall be of reasonable permanent nature and where feasible shall be "tied" by survey to other objects of a permanent nature and the "ties" shall be shown on said map. The map shall show approximate location of any property line within 200 feet of the proposed right-of-way.

Applicant may, in initial application, propose a name for the road, but the Selectboard will determine the road name. The State Agency of Transportation will assign a number to the road.

(See appendix 6.04 for the application form.)

3.03 FEES

A fee as per 32 V.S.A. Section 1671 shall accompany each application to cover the cost of bookkeeping and recording.

A fee per sign will be charged to the owner at cost to cover the cost of purchase, labor and materials to install each sign. Signs, as approved by the Selectboard, will be billed to the owner and payment must be received before final acceptance.

3.04 COMPENSATION FOR DAMAGES

All damages to the road, right-of way, bridges, culverts, streams, wet lands, utilities, and related structures and landscape caused by road construction by the applicant or by his negligence are the responsibility of the applicant and shall be repaired under the supervision of the Selectboard or their representative and at the expense of the applicant. Failure to repair said damage will result in the Town completing the work and billing the applicant.

3.06 INSPECTION (Refer to Section 4.00 - Minimum Standards)

All road designs, including cut and fill plans, gravel pit plans, surveys, layouts, right-of-way, utility locations, road construction site, preparation and construction shall be inspected by the Selectboard or their representative.

The Selectboard or their representative shall inspect the road site as follows:

1. Before clearing
2. Before stumping
3. After rough grading
4. Upon completion of subgrading
5. During graveling
6. Before paved surface treatment
7. Final inspection

The developer shall have stakes set on centerline with finish grades marked on them at least every 100 feet for the entire length of the road or the road side staked with distance and grades marked before each inspection.

Before final inspection, the developer shall remove all trash from the right-of-way and shall repair any damage done to the roadway, shoulders, drainage structures and related road items. All slopes will be seeded and mulched.

Inspections, samples and core test may be taken by the Town before final acceptance and all costs incurred shall be reimbursed by developer.

The Town requires that all materials may be sampled during construction and must comply with Vermont Agency of Transportation specifications.

3.07 COMPLETION DEADLINE

No roads will be inspected or accepted from October 15th to April 15.

Road specifications in effect at the time of the initial inspection will be applicable for a period of two years from that date after which time any new road specifications will apply. The only exception will be if the road in question at the end of the two year period in Section 3.08 is under the 12 month maintenance period.

3.08 ACCEPTANCE

When the road has been completed, inspected, and one house per quarter mile of new road has been completed, and occupied by full-time resident(s) the Town will, upon request, begin normal maintenance for 12 months.

After 12 months of maintenance, if no serious defects have been observed, the deed will be recorded and the road will become a Town road. During the 12 months initial acceptance period, any flaws or defects which are pointed out to the road builder will be his responsibility to correct. During this period, the Town will order and erect the necessary signs.

3.09 CONVEYANCE

All rights-of-way, discharge rights and slope rights shall be conveyed to the Town of Stamford by warranty deed in fee simple and free of all encumbrances. The Town will have the right to leave the right-of-way to maintain the road.

SECTION 4.00 - MINIMUM STANDARDS

Roads vary in their functional use. The standards by which a road is constructed are important to its proper functioning. As the quantity of traffic in a road increases, so must the standards change. Consequently, the standards which follow are considered minimum standards and are subject to modification by the Selectboard after a study of local conditions requiring changes.

Requests for waiver of any portion of these standards shall be made in writing to the Selectboard.

Vermont Agency of Transportation Standards B-71 in its entirety and A-23 for banking, ditch, backslope and guard rail specifications are hereby adopted and enforced as supplemental criteria under this ordinance.

Wherever feasible the road bed or surface should be above the surrounding terrain to allow for drainage and snow removal.

4.01 RIGHT-OF-WAY WIDTH

The right-of-way shall be minimum of 50 feet in width with wider slope and drainage rights if deemed by the Selectboard. The center line of the road is to be located in the center of the right-of-way. The right-of-way conveyed shall intersect at least one existing Class 1, 2, 3 or State Highway.

4.02 TRAVELLED WAY WIDTH

The minimum traveled way width of a road shall be 20 feet not including shoulders.

4.03 CLEARING

Right-of-way is to be cleared a minimum of 40 feet of all trees, brush and stumps. Burning shall be done in accordance with State and Town fire regulations. No fallen trees shall remain in the right-of-way. All stumps shall be removed, so that base of road is on hardpan, gravel, ledge or properly compacted fill.

4.04 SUBGRADE

Boulders shall be removed to at least 18 inches below subgrade and replaced with gravel with stones no larger than 6 inches.

All loam, muck, stumps and other improper foundation material shall be removed from within the limits of the fully extended road shoulders. In embankment areas, suitable foundation material shall be placed in one foot layers and compacted to form a stable subgrade.

The subgrade surface shall conform exactly in cross section and grade to the finished road surface. Crowning (see section 4.10) and banking (Vermont Standard Specifications for Construction A-23) of curves shall be as required by the Selectboard. Compaction is required on any portion of the subgrade which has been disturbed by excavation or which has been filled during the construction of the subgrade. All ditches and drains will be constructed so that they effectively drain the subgrade prior to the placement of any base material. An additional 6 inches of gravel with stone no larger than 3 inches shall be placed over any clay subgrade.

4.05 WET AREA

If the proposed road is in a wet area, Selectboard may require additional gravel and underdrain to insure a stable road.

4.06 SUBBASE

The subbase shall be composed of a minimum of 18 inches of compacted gravel or crushed stone with maximum stone size of 4 inches. The cross section and grade of the subbase shall conform to those of the finished surface.

4.07 UPPER BASE

All roads will receive a 6 inch upper base course of compacted crushed gravel with a maximum stone size of 1 1/2 inches as defined by the Vermont Standard Specifications for Construction.

4.08 SHOULDERS

Shoulders shall be a minimum of two feet in width on a side and shall be constructed to the same specifications as the subgrade, subbase and base. Shoulders shall be a minimum of four feet at guardrail sites.

4.09 PAVING

The road is to be paved within a year after construction and after one winter season has passed with a hard surface (oil and stone, cold mix, or hot bituminous concrete) meeting all of Vermont State Highway specifications.

1. Oil and Stone is to be laid in two layers (double coating) with the first layer having stones no less than 3/4 inch and the second layer having chip stones no less than 3/8 inch.
2. Cold mix is to be laid with at least a 2 inch layer in a single application.
3. Hot bituminous concrete is to be laid and rolled by experienced crews in two layers. The first, the binder course, to a depth compacted to 2 inches and the second application compacted to a depth of 1 inch of hot bituminous concrete, surface mix.

Future foreseeable connection or intensifications of a proposed road shall be considered by the Selectboard in determining what form of pavement or finishing is required.

4.10 CROWN

The road shall have a cross slope from centerline to edge of shoulder of 1/4 inch per foot and from the edge of road to edge of shoulder 1/4 inch per foot.

4.11 DITCHES, CULVERTS AND HEADERS

Drainage ditches shall be provided where necessary and shall be constructed to prevent infiltration of water into the gravel subbase and to conduct storm drainage to waterways and absorption areas. Accordingly, drainage ditches adjacent to roads are normally to be at least 6 inches below the gravel subbase or 18 inches below finished grade to minimize spring breakup conditions. Ditches shall be shaped to prevent excessive erosion on both shoulder and right-of-way or bank sides of the ditch cross section.

Open drainage ditches in excess of 5% grade or in heavy water areas shall be paved with stone or asphalt.

Underdrain will be required where soil and water conditions make it desirable.

Culverts shall be installed during the construction of the road and prior to road subbase and surface preparation and placement. Backfill in excavations for culverts shall be 1 1/2 inch gravel compacted to prevent or minimize settling in surface, shoulders or slopes. Culverts should be covered with a minimum of 36 inches of material.

Headers shall be installed at the inlet of all culverts and may be either reinforced concrete 8 inches thick, large flat rocks tightly placed, or large cemented rock. Marker posts 4 to 6 inches in

diameter shall be installed at both ends of the culvert.

Culverts shall be installed in all low spots and be of sufficient size to handle the anticipated run-off but shall not be less than 15 inches in diameter. The culverts shall extend at least 1 foot beyond all fill and shall have not less than 3 feet of packed cover. The ground will be lower than the culvert on the exit end to allow leaves and debris to drain off.

Culverts shall be spaced no greater than 300 feet apart in one continuous ditch line.

4.12 SLOPES AND BANKS

Slopes or banks shall be designed and constructed to prevent instability, slides, washes, or other disturbance to the slope or bank surface or subsurface. Banks shall not interfere with snow removal. After construction and final grading of banks, they will be seeded and mulched to minimize surface erosion. Cribbing or riprap shall be installed where necessary.

4.13 GUARD RAILS

Approved types of guardrails and posts will be installed to meet essential traffic control and safety needs, and shall not interfere with snow removal. Normally, any road with a slope or bank falling away on a steep slope (4:1 or steeper) from the road surface, and which is 5 feet or higher in elevation, shall be protected with guard rail unless such requirement is waived by the Town in approving the road plan and application. Guard posts and rails shall also be installed to warn and protect traffic from ledge or other obstructions where necessary in consideration of traffic volume, road width, and safety. Installation shall conform with Vermont Agency of Transportation requirements.

4.14 GRADES

Road grades shall be at least 1% but not more than 10%. Finished grades (transverse and longitudinal) shall be smoothed to eliminate sharp dips in traveled surface and, as may be necessary, to permit efficient snow removal and proper drainage.

The maximum grade within 50 feet of an intersection shall be 4%.

4.15 CURVES

Radius of curves shall be long enough to permit easy flow of traffic, including trucks, graders, and fire engines, with at least a 100 foot radius in all cases, measured in the center line. Trees and boulders shall be removed to permit adequate sight on all curves. Whenever possible, all curves will be straightened and blind areas will be eliminated.

4.16 DRIVEWAYS, ENTRANCES AND APPROACHES

The property owner who wishes to build a driveway shall obtain a permit for the driveway from the Road Commissioner of the Town of Stamford. Permit is valid for one (1) year but can be

extended for one (1) year upon written request to the Road Commissioner.

Upon application for a permit to build a driveway, the owner shall submit, in writing, plans for the driveway, including but not limited to location, drainage, width and grade, all of which must meet the approval of the Selectboard's agent, the Road Commissioner.

Upon completion of the driveway for which a permit was obtained, the Road Commissioner shall inspect the driveway. If the driveway does not meet with the Road Commissioner's approval, the owner shall be deemed to be in violation of this ordinance.

Driveways cannot interrupt the natural or ditch line flow of drainage water. In some cases where shallow ditch lines or natural drainage courses exist, driveways may be swelled at a point beyond the road shoulder to accommodate the flow of storm water. In all other case, driveways must have sufficiently sized culverts installed and forever maintained by the homeowner or developer. Driveways are to be constructed in accordance with the Vermont Standard Specifications for Construction B-71.

In no case shall the culvert pipe under a driveway be less than a 15 inch diameter pipe.

An all season safe sight distance of 200 feet in each direction must be present for a driveway building permit to be issued.

Driveways shall intersect the roadway at a preferred angle of 90 degrees but in no case shall the intersecting angle be less than 50 degrees.

No driveway will be permitted to be constructed within 100 feet of an intersecting road and 150 feet is desirable.

Under no circumstances shall a driveway permit allow construction which will result in drainage or washing directly out to a town road.

Any and all damage done to town property by any person or vehicle pertaining to this permit will be repaired by the applicant, landowner or contractor mentioned in this permit to the full approval of the town. All material to be used, gravel, pipe, etc will meet the town's specifications and approval.

In the event a driveway causes damage to a town road through improper construction, maintenance, or grading, it shall be the responsibility of the property owner to make necessary repairs upon notification in writing by the Town. In the event such repairs that are required are not made within 30 days, the Town shall take whatever steps are necessary to insure the interests of the Town and shall bill the property owner for any expenses involved.

(See appendix 6.02 for the application form.)

4.17 INTERSECTIONS

Intersections shall be as nearly as possible at right angles (90 degrees) with a minimum allowable intersection angle of 60 degrees.

The centerline of no more than two accepted right-of-ways shall intersect at any one point.

Any intersections of two roads with a third road shall be separated by a distance of not less than 150 feet between center lines.

4.18 CUL-DE-SACS AND DEAD END ROADS

Dead end roads are discouraged. Cul-de-sacs will be permitted but not of greater length than 1200 feet and must have a turn-a-around with a minimum diameter of one hundred feet built to subgrade, subbase and upper base specifications. Provisions shall be made at the perimeter for snow removal.

Roads shall be arranged to provide for extension or connection of eventual road systems necessary to develop abutting land in future subdivisions.

4.19 TURNOFFS

Turnoffs with adequate elevations, surface, drainage ditches, and culverts will be provided to permit safe passing under summer and winter conditions, and shall be dimensioned and constructed to enable effective and efficient snow removal.

4.20 ROADSIDES

At completion of the project, excess debris not covered, shall be removed from the right-of-way. Burial of wood, such as stumps will be permitted within the right-of-way outside the traveled portion.

All disturbed portions of the roadsides shall be loamed, seeded and mulched.

4.21 UTILITIES

All underground electric, telephone and cable TV distribution systems within subdivisions shall be placed out of the Town's right-of-way.

The developer shall coordinate designs with the appropriate utility companies to insure adequate and suitable area for underground installations out of the Town's right-of-way.

The developer shall be responsible for providing for water mains, manholes, sanitary sewers and catch basins if required.

Easements across lots or centered on rear of side lot lines shall be provided for utilities where

necessary and shall be at least 20 feet wide.

4.22 CURBS AND SIDEWALKS

These regulations do not require construction of sidewalks or curbs or the setting of edgestones. When required, however, such as near schools and designated activity areas, they shall be constructed or set in accordance with the policy and specifications agreed upon with the Board of Selectboard.

4.23 SIGNING

Street signs shall be in conformance with the Manual on Uniform Traffic Control Devices (MUTCD) and paid for by the developer.

4.24 BRIDGES

Plans for bridges must be submitted for review prior to construction. Construction will be authorized only after a review by the State Agency of Transportation indicates that the bridge will meet all applicable Vermont Agency of Transportation Specifications.

4.29 SPEED LIMITS

Upon completion of a traffic engineering survey made by the Stamford Selectboard speed limits for the roads in Stamford will be posted.

Speed limits effective July 22, 1996

Basin Rd.	TH #6	25 mph
Brookside Dr.	TH #27	25 mph
Bushika Rd.	TH #28	25 mph
Clayton Ave.	TH #22	25 mph
Clough Rd.	TH #25	25 mph
Collins Rd.	TH #4	25 mph
East Rd.	TH #1	
Clough Rd. to Haggerty's Bridge		25 mph
Haggerty's Bridge to MA line		30 mph
Henry Hill	TH #23	25 mph
Jepson Rd.	TH #2	25 mph
Lesure Rd.	TH #16	25 mph
Lincoln Rd.	TH #7	25 mph
Maltese Rd.	TH #10	25 mph
Mill Rd./County Rd.	TH #12	25 mph
Old County Rd.	TH #11	25 mph
Robillard Rd.	TH #26	25 mph
Sparrow Dr.	TH #31	25 mph
Sportsman Rd.	TH #3	25 mph

Stebbins Ln.	TH #20	25 mph
Sumner Rd.	TH #8	25 mph
Tatro Rd.	TH #9	25 mph
The Lane	TH #19	25 mph

4.30 ON-STREET PARKING

No person shall leave a motor vehicle on any town road so as to interfere with snow removal from November 1st until April 1st of each year. A motor vehicle so illegally parked may be towed away at the full responsibility of the owner for both the towing and storage charges and for any damages resulting to the vehicle due to such towing.

4.36 STREET SIGNING

Pursuant to the authority granted by Title 23 V.S.A. Section 1008 the Selectboard of the Town of Stamford, Vermont do hereby adopt the following street signs.

STOP SIGNS

A person operating any motor vehicle defined by No. 15 of definitions in Title 23 V.S.A. Section 4, shall cause such vehicle to come to a complete stop before entering:

The intersection of Town Highway No. 21 (Cemetery Road/Millard Road) with Route 8/100.

The intersection of Town Highway No. 19 (The Lane) with Route 8/100.

The intersection of Town Highway No. 19 (The Lane) with Town Highway No. 1. (East Road)

The intersection of Town Highway No. 2 (Jepson Road) with Route 8/100.

The intersection of Town Highway No. 17 (Boulger Road) with Town Highway No. 2. (Jepson Road)

The intersection of Town Highway No. 20 (Stebbins Lane) with Route 8/100.

The intersection of Town Highway No. 12 (Mill Road) with Route 8/100.

The intersection of Town Highway No. 23 (Henry Hill) with Town Highway No. 12. (Mill Road)

The intersection of Town Highway No. 9 (Tatro Road) with Town Highway No. 12. (Mill Road)

The intersection of Town Highway No. 31 (Sparrow Drive) with Town Highway No. 12.

(County Road)

The intersection of Town Highway No. 10 (Maltese Road) with Town Highway No. 12.
(County Road)

The intersection of Town Highway No. 22 (Clayton Avenue) with Route 8/100.

The intersection of Town Highway No. 11 (Old County Road) with Route 8/100.

The intersection of Town Highway No. 8 (Sumner Road) with Town Highway No. 11.
(Old County Road)

Both the intersections of Town Highway No. 25 (Clough Road) with Route 8/100.

The intersection of Town Highway No. 1 (East Road) with Town Highway No. 25.
(Clough Road)

The intersection of Town Highway No. 18 (Forgette Road) with Town Highway No. 1.
(East Road)

The intersection of Town Highway No. 29 (Shippee Driveway) with Route 8/100.

The intersection of Town Highway No. 7 (Lincoln Road) with Route 8/100.

Both the intersections of Town Highway No. 26 (Robillard Road) with Route 8/100.

The intersection of Town Highway No. 6 (Basin Road) with Town Highway No. 26.
(Robillard Road)

The intersection of Town Highway No. 27 (Brookside Drive) with Route 8/100.

The intersection of Town Highway No. 4 (Collins Road) with Route 8/100.

Both the intersections of Town Highway No. 28 (Bushika Road) with Route 8/100.

The intersection of Town Highway No. 24 (Rondeau Road) with Town Highway No. 28.
(Bushika Road)

The intersection of Town Highway No. 3 (Sportsmen Road) with Route 8/100.

LEGAL LOAD 24,000 POUNDS

No person may drive a vehicle in excess of 24,000 pounds on the following enumerated town highways:

Town Highway No. 21 (Cemetery Road/Millard Road)

Town Highway No. 19 (The Lane)
Town Highway No. 2 (Jepson Road)
Town Highway No. 17 (Boulger Road)
Town Highway No. 16 (Lesure Road)
Town Highway No. 20 (Stebbins Road)
Town Highway No. 12 (Mill Road)
Town Highway No. 23 (Henry Hill)
Town Highway No. 9 (Tatro Road)
Town Highway No. 12 (County Road)
Town Highway No. 31 (Sparrow Drive)
Town Highway No. 10 (Maltese Road)
Town Highway No. 22 (Clayton Avenue)
Town Highway No. 11 (Old County Road)
Town Highway No. 8 (Sumner Road)
Town Highway No. 25 (Clough Road)
Town Highway No. 1 (East Road)
Town Highway No. 18 (Forgette Road)
Town Highway No. 29 (Shippee Driveway)
Town Highway No. 7 (Lincoln Road)
Town Highway No. 26 (Robillard Road)
Town Highway No. 6 (Basin Road)
Town Highway No. 27 (Brookside Drive)
Town Highway No. 4 (Collins Road)

5.06 PUBLICATION AND POSTING

This ordinance shall be published in the North Adams Transcript on February 3, 1994, and was filed with the Stamford Town Clerk on January 27, 1994.

Adopted by the Board of Selectboard, Town of Stamford at its meeting held on January 27, 1994, and shall become effective sixty (60) days from that date on March 28, 1994.

Amended May 23, 1996:

4.29 to include the speed limits. Effective July 22, 1996.

Amended December 3, 1998:

1. Added the third paragraph, "This ordinance is designated as a civil ordinance pursuant to 24 V.S.A. 1971(b). A violation of this ordinance shall be a civil matter enforced in accordance with the provision of 24 V.S.A. 1974 (a) and 1977 et seq."

2. Added Section 2.10

Amended May 13, 2010

#1. Changed all "selectmen" to "selectboard."

#2. SECTION 2.05 Class 4 Roads. Added paragraph 2 prohibiting wheeled vehicles during adverse conditions; requirements for temporary permits for heavy equipment or plowing. Added paragraph 3 closing of class 4 roads and plowed by permission only.

#3. SECTION 2.08 Posting. Added to second paragraph posting for the purpose of preserving the integrity of the highway.

#4. SECTION 2.10 Laying of Utilities in Town Highways. Added to first paragraph for expiration date on permit. Added last paragraph for responsibility to maintain and repair damages.

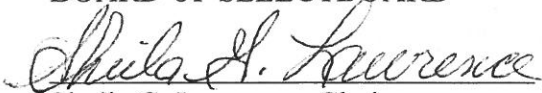
#5. SECTION 4.16 Driveways, Entrances and Approaches. Added to first paragraph for expiration date on permit. Added paragraph #10 for responsibility to repair damage to town property.

#6. Appendix 6.02 Insert expiration date on permit.

#7. Appendix 6.03 Responsibility to maintain and repair damages, verification statement, insert expiration date on permit, and traffic control fee.

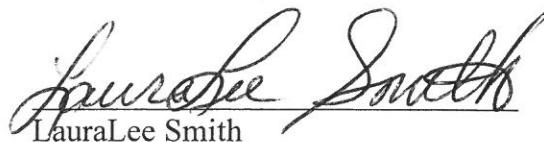
Readopted the entire ordinance with the new amendments on May 13, 2010, and shall become effective (60) days from that date on July 12, 2010.

BOARD OF SELECTBOARD


Sheila G. Lawrence, Chairman


Eunice M. Rice


Thomas V. Houghtaling


LauraLee Smith

Town Highway No. 28 (Bushika Road)

Town Highway No. 24 (Rondeau Road)

Town Highway No. 3 (Sportsmen Road)

4.37 OPERATION OF VEHICLES

All provisions of Title 23 of The Vermont States Annotated are hereby adopted.

SECTION 5.00 - GENERAL PROVISIONS

5.01 SEPARATE OFFENSES

Each violation of a provision of this ordinance shall be deemed a separate offense.

5.02 PENALTIES

The provisions of this ordinance shall be cumulative to the fullest extent permitted by law with respect to all other statutes or ordinances now or hereafter adopted regardless of their order of passage or enactment.

5.03 SEVERABILITY

The provisions of this ordinance are declared to be severable and if any provision hereof be adjudged invalid such judgement shall not affect the validity of any other provision.

5.04 DESIGNATION

This ordinance may be referred to as the Stamford Town Highway Ordinance and in a prosecution hereunder a copy of such ordinance, certified by the Town Clerk shall be prima facie evidence thereof. An allegation that the act constituting the offense charged is contrary to a specified provision of this ordinance shall be a sufficient reference hereto.

5.05 REPEAL OF PRIOR ORDINANCES

Any previous ordinance, policy or regulation heretofore adopted by the Town of Stamford which conflicts with this ordinance is hereby repealed.

Driveway Permit # _____ Permit Fee: \$ \$20.00 & Recording Fee: \$ \$10.00 = Total of \$30.00.
Original Document with any attachments is filed in Road Right of Way File in the Town Office

DRIVEWAY DESIGN: Date: _____ Approved with requirements** _____ Denied: _____
Appealed: _____

DRIVEWAY INSTALLATION: Date: _____ Approved: _____ Denied: _____
Appealed: _____

-----ABOVE FOR OFFICE USE ONLY-----

TOWN OF STAMFORD

986 Main Road, Stamford, VT 05352 (802) 694-1361

Driveway/Road Right of Way Permit

and Memorandum of Municipal Action 24 VSA section 4443 (c)

SECTION I

1. Location of Property: _____ District: _____
Deed Recording: Book #: _____ Page #: _____ Parcel ID #: _____

2. Legal name of landowner/applicant: _____
(list all owners of record as shown in deed)
Mailing address: _____ Tel #: _____

3. Name of contractor: _____ Tel #: _____
Mailing address: _____

- 4. Purpose:
 - a. construct new driveway _____
 - b. change existing driveway _____
 - c. disturb roadway / change roadside border _____
 - d. temporary right of way access _____
 - e. culvert size, if needed _____
 - f. to lay underground wiring _____

Anticipated date of starting work: _____ Anticipated date of completion: _____

** REQUIREMENTS: _____

Any and all damage done to town property by any person or vehicle pertaining to this permit will be repaired by the applicant, landowner or contractor mentioned in this permit to the full approval of the town. All material to be used, gravel, pipe, etc will meet the town's specifications and approval.

SECTION II

I swear under the pains and penalties of perjury that the statements contained in this application are true to the best of my knowledge and belief. Permit is valid for one (1) year but can be extended for one (1) year upon written request to the Road Commissioner. Submission of this document authorizes the Road Commissioner to view and inspect the property before, during and upon completion of the project for which this permit is granted.

Date: _____ Signature of Applicant: _____

NOTE: Failure to provide any of the above required information will result in this application being returned to the applicant.

Application received by _____ Date received in town office _____ Fee _____
(does not constitute receipt by Road Commissioner)

***** FOR USE OF ROAD COMMISSIONER *****

Driveway Permit number: _____ Date received by Road Commissioner: _____

DRIVEWAY DESIGN: Date: _____ Approved with requirements ** _____ Denied: _____

Appealed: _____

Signature of Road Commissioner: _____
David R. Tatro

DRIVEWAY INSTALLATION: Date: _____ Approved: _____ Denied: _____ Appealed: _____

Signature of Road Commissioner: _____
David R. Tatro

Permit is not valid until site is viewed and permit is signed by the Road Commissioner

Utility Permit # _____ Permit Fee: \$ \$20.00 & Recording Fee: \$ \$10.00 = Total of \$30.00.
Date: _____ Permit Approved: _____ Denied: _____ Appealed: _____
Original Document with any attachments is filed in Utilities in Town Highway File in the Town Office
-----ABOVE FOR OFFICE USE ONLY-----

TOWN OF STAMFORD

986 Main Road, Stamford, VT 05352 (802) 694-1361
PERMIT FOR LAYING OF UTILITIES IN TOWN HIGHWAYS
and Memorandum of Municipal Action 24 VSA section 4443 (c)

Location of Property: _____ Road Name: _____ Parcel ID # _____

Legal Name of landowner/applicant:

(list all owners of record as shown in deed)

1. Road surface will be cut to provide a straight edge.
2. All material in the traveled portion of the road to the full depth of the ditch (6 feet minimum) will be removed and replaced with 1 ½ inch crushed gravel or equivalent approved by the selectmen or their representative. All other road crossings, i.e. borings, etc. will be a minimum of 6 feet unless otherwise approved by the selectmen or their representative.
3. The utility to be installed will be sleeved in 4 inch schedule 40 minimum for the full width of the town highway.
4. Trench will be filled in 1 foot intervals and compacted by mechanical means, i.e. Wacker, Jumping-Jack etc., in traveled portion of highway.
5. There will be a minimum of 4 inches of hot asphalt to complete trench in traveled portion of highway.
6. All other town property will be restored to its present condition, ditches, roadsides, etc. within seventy-two (72) hours from date of excavation.
7. Job will be properly signed and due to liability to the town, a representative of the town, either the road commissioner, constable or duly appointed representative of the selectmen, will be present for traffic control until the highway is restored. Payment for their time will be paid by the permit holder.
8. Permit holder will be responsible for road surface sagging and/or deterioration for one year from date trench is completed.
9. No work will start in town's right of way until permit holder agrees upon all conditions of permit and all permits and fees are in place. This permit is for one day. Ditch must be closed or plated at end of workday. If additional days are needed then a permit must be obtained for each additional day.
10. The town will be notified a minimum of three work days prior to date and time of proposed project so coordination may take place between proper authorities.
11. Upon completion of laying of utilities of which a permit was obtained, the Road Commissioner shall inspect the site. If it does not meet with the Road Commissioner's approval, the owner shall be deemed to be in violation of the Stamford Town Highway Ordinance.

12. It is the land owner's responsibility to maintain and repair and any all future damages caused by the laying of underground utilities in the Town Right of Way.

I swear under the pains and penalties of perjury that the statements contained in this application are true to the best of my knowledge and belief

Permit is valid for one (1) year but can be extended for one (1) year upon written request to the Road Commissioner. Submission of this document authorizes the Road Commissioner to view and inspect the landowner's property before, during and upon completion of the project for which this permit is granted.

Submitted and agreed upon by: _____ Date: _____

Tel #: _____ Date of Proposed Excavation: _____

APPROVED BY: _____ DATE: _____

David R. Tatro, Road Commissioner
(Permit not valid until site is viewed and permit signed by the Road Commissioner)

Fee of \$ _____ taken by _____ on _____. Permit fee \$20.00 and recording fee \$10.00 (both non-refundable) and traffic control fee of \$ 200.00 (unexpended amount, if any, to be refunded)

APPENDIX 6.04

APPLICATION FOR ROAD UPGRADE
OR
TOWN ACCEPTANCE OF A PROPOSED NEW ROAD

NAME OF APPLICANT: _____

STREET ADDRESS: _____

CITY, STATE, ZIP CODE: _____

PHONE NUMBER: _____

This application is for:

_____ Permission to upgrade Town Highway No. _____

_____ A proposed new road extending from existing Town Highway No. _____.

The length of the road to be constructed or upgraded is _____

Estimated cost of construction is _____

The applicant agrees as a condition governing the issuance of the Selectboard's permission to upgrade the above road, that he will hold harmless the Town of Stamford, its agents and employees from any and all claims and actions whatsoever arising from the exercise of said permission.

SIGNATURE OF APPLICANT

DATE

This application must be delivered to the Stamford Town Clerk along with the following:

1. Three copies of a survey map of the existing road prepared by a registered surveyor based on the original recorded description and a survey based on the existing road bed,
2. Three copies of a plan of the proposed upgrade showing grade lines, plan and profile of the road and drainage layout (location and size),
3. A proposed warranty deed of the land to be conveyed for road purposes,
4. A list of the names of all property owners who abut this portion of the road purposes. The grade line and minimum radii of all curves must be shown.
5. A fee as described in Section 3.03 of the Stamford Highway Ordinance.

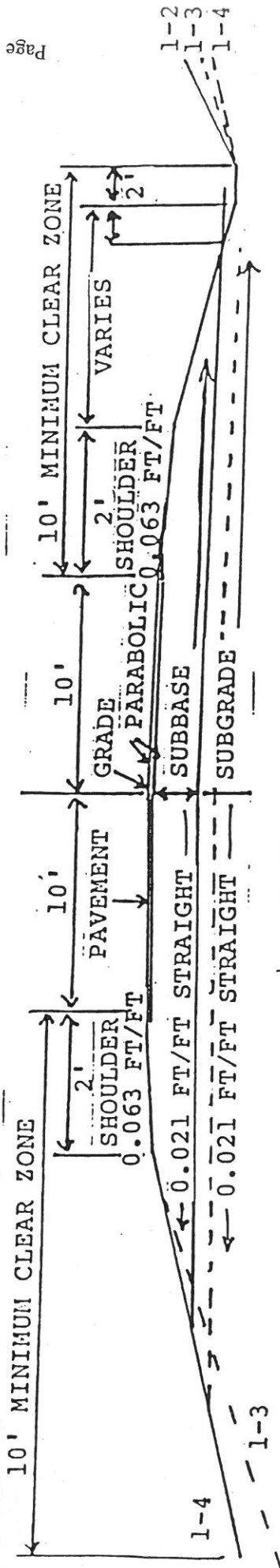
This application and any subsequent permission will be void in the event of misrepresentation of information supplied.

PAVEMENT: Oil and Stone or
 2" Cold Mix or
 3" Hot Bituminous Concrete

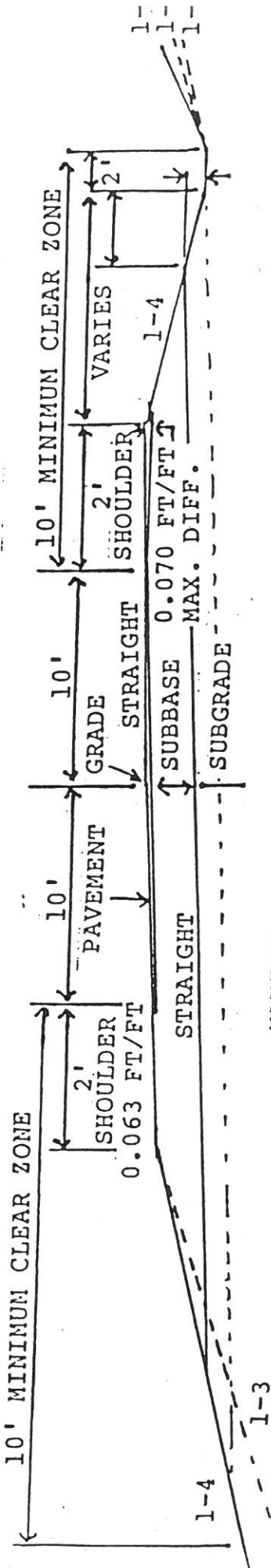
UPPER BASE: 6" compacted stone with stones no larger than 1 1/2".
 SUBBASE: 18" compacted stone with stones no larger than 4".
 SUBGRADE: 18" compacted stone with stones no larger than 6".

I hereby certify that this is a true copy of the Stamford Town Highway Ordinance.

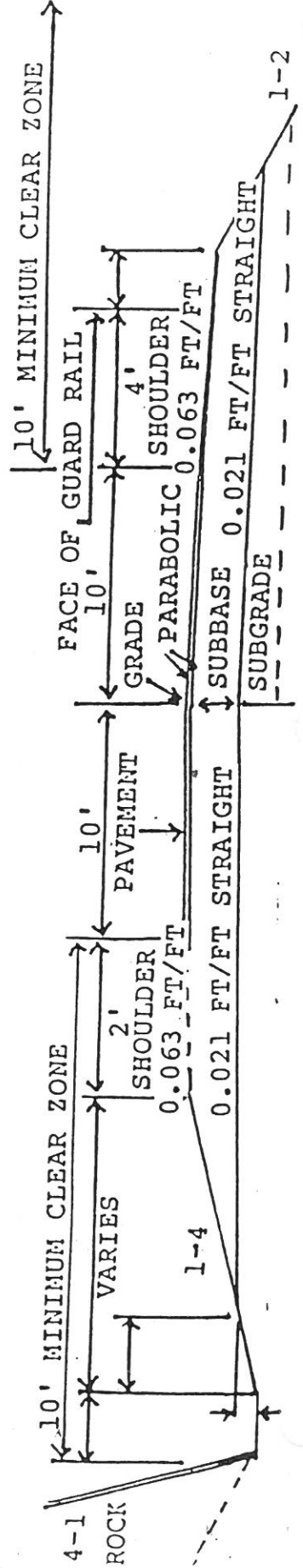
Attest: _____
 Lori A. Shepard, Town Clerk



NORMAL SECTION



MAXIMUM BANKED SECTION FT/FT



NORMAL SECTION IN ROCK CUT

NORMAL SECTION WITH GUARD RAIL

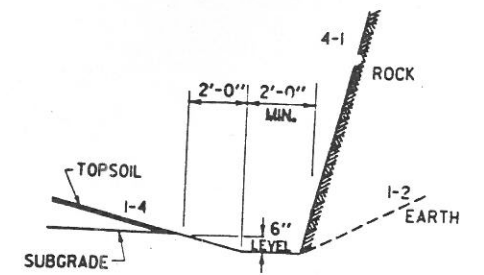
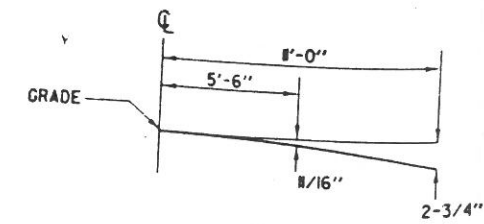
TYPICAL SECTIONS

MATERIAL ITEM	THICKNESS TOLERANCE
PAVEMENT	* 1/4"
SUBBASE	* 1"
GRANULAR BORROW	* 1"
SAND BORROW	* 1"

1 1/2" TO 2" BITUMINOUS CONCRETE PAVEMENT

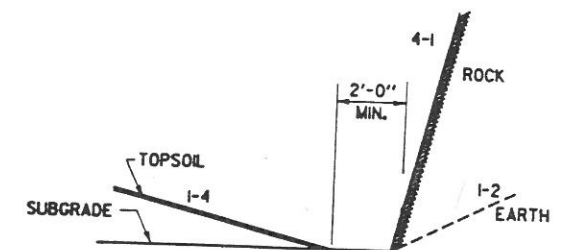
18" SUBBASE OF GRAVEL

SHOULDERS : BITUMINOUS CONCRETE PAVEMENT (AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER)
 0" TO 12" GRANULAR BORROW (IN FILLS) } AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER
 0" TO 24" SAND BORROW (IN CUTS)

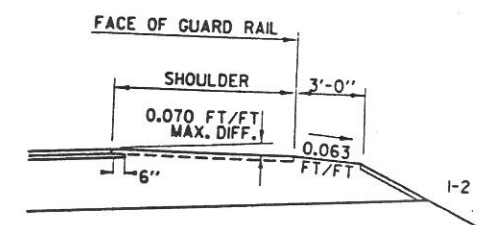


DETAILS OF DITCH AND BACKSLOPE FOR LOW SIDE OF BANK < 0.042 FT/FT

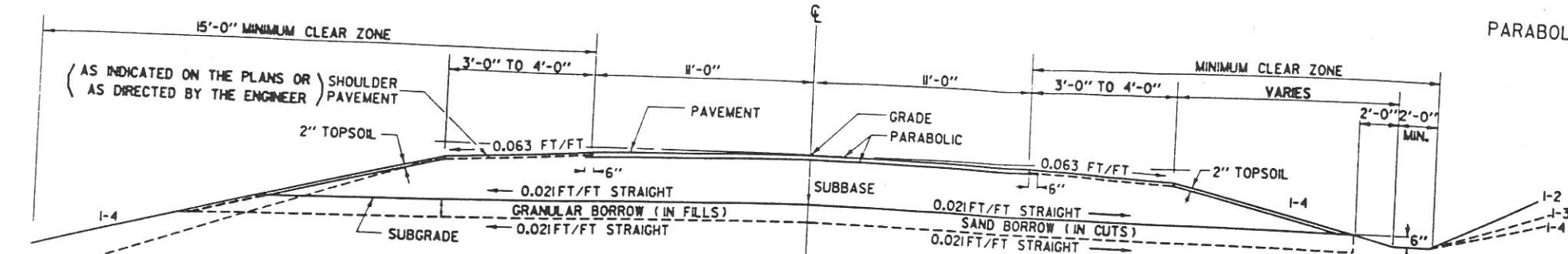
FOR SLOPES IN SOLID ROCK EXCAVATION AND DRILLING AND BLASTING OF SOLID ROCK SUBGRADE, SEE STANDARD SHEET A-6L.



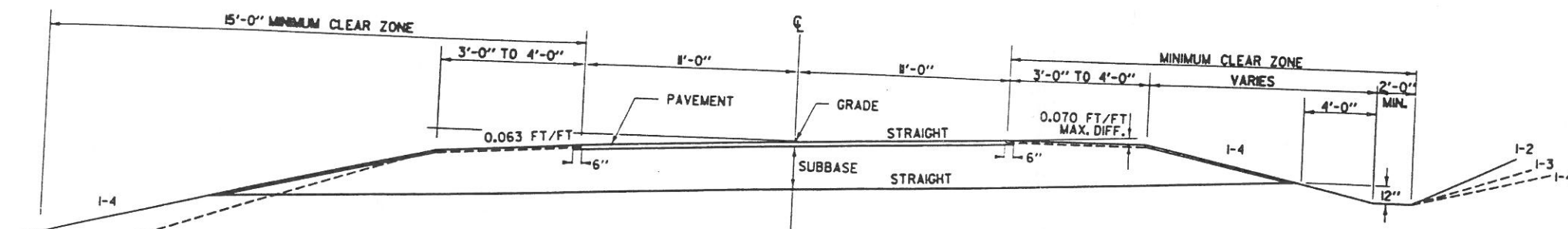
DETAILS OF DITCH AND BACKSLOPE FOR LOW SIDE OF BANK >= 0.042 FT/FT



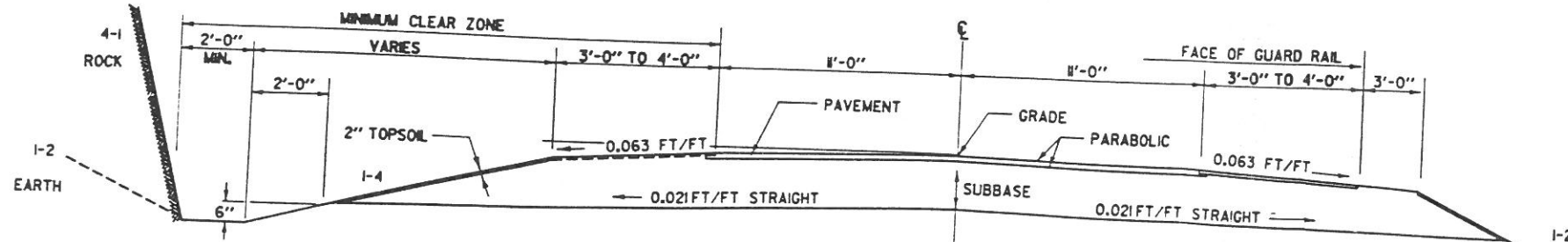
DETAIL OF GUARD RAIL ON HIGH SIDE OF BANKED SECTION



NORMAL SECTION



MAXIMUM BANKED SECTION FT/FT



NORMAL SECTION IN ROCK CUT

NORMAL SECTION WITH GUARD RAIL

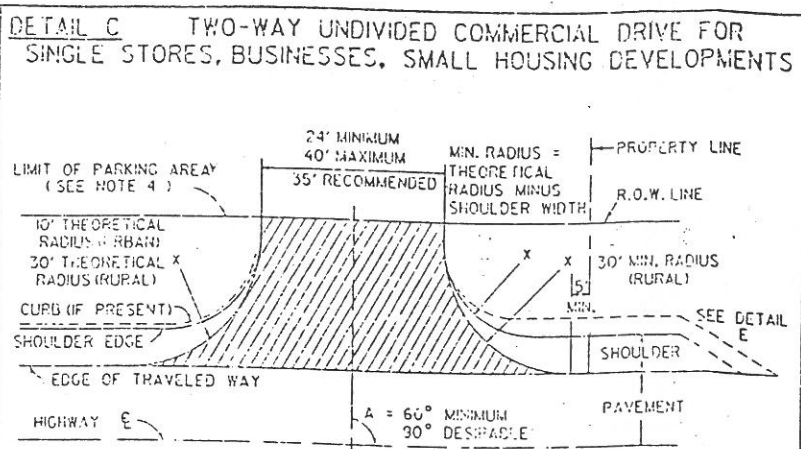
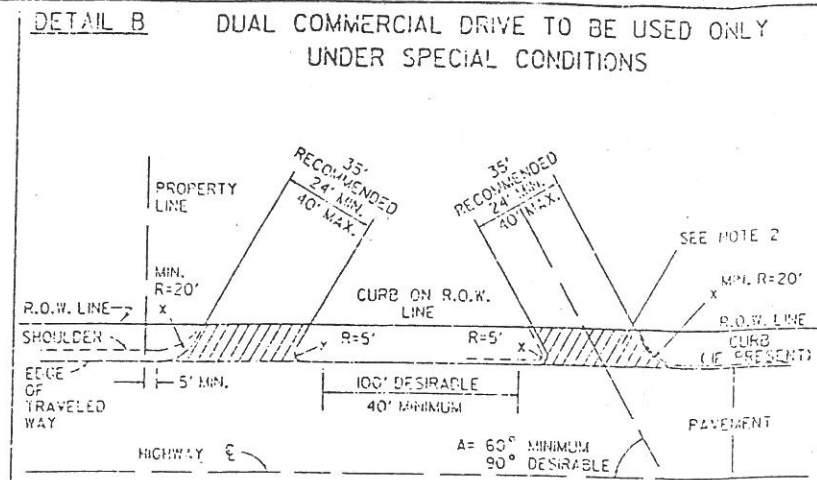
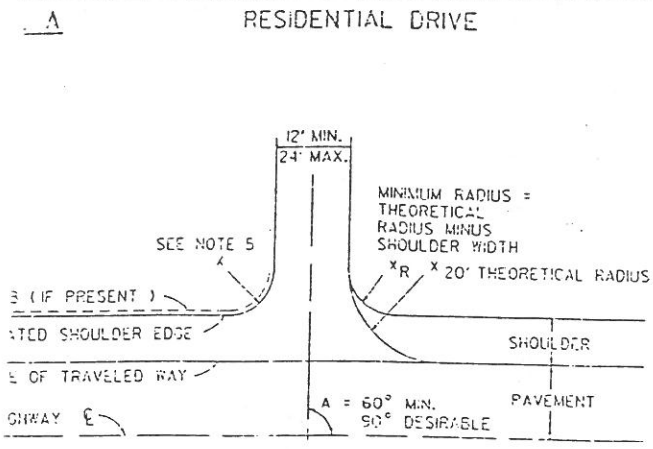
REVISIONS AND CORRECTIONS

APPROVED	DATE
_____	APRIL 2, 1986
_____	DATE
_____	CHIEF ENGINEER
_____	CHIEF OF DESIGN
_____	SURVEY AND PLANS ENGINEER

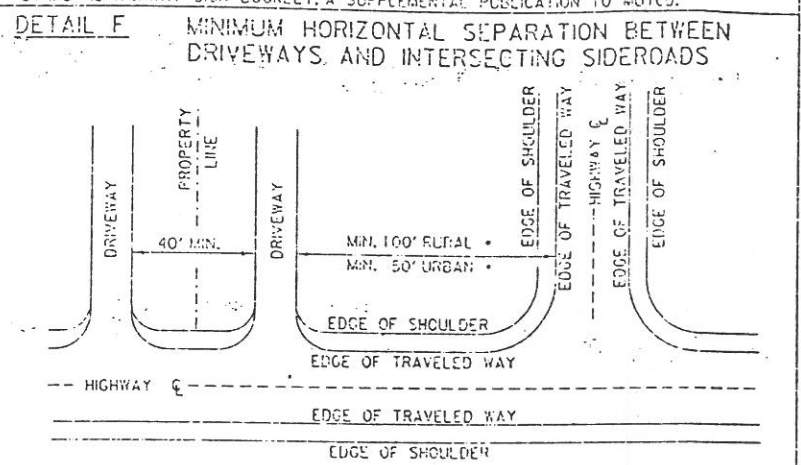
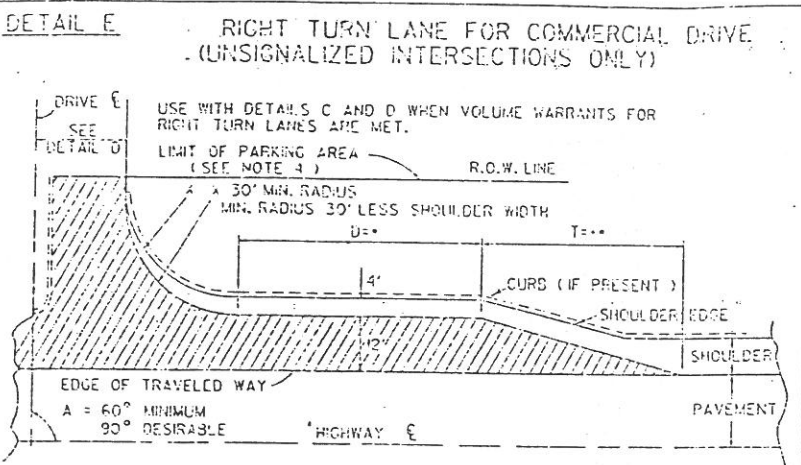
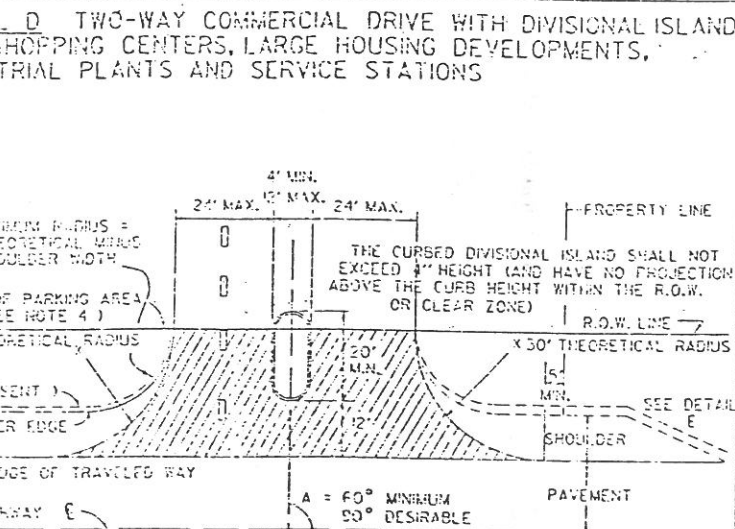
RURAL HIGHWAY TYPICAL SECTIONS



STANDARD A-23



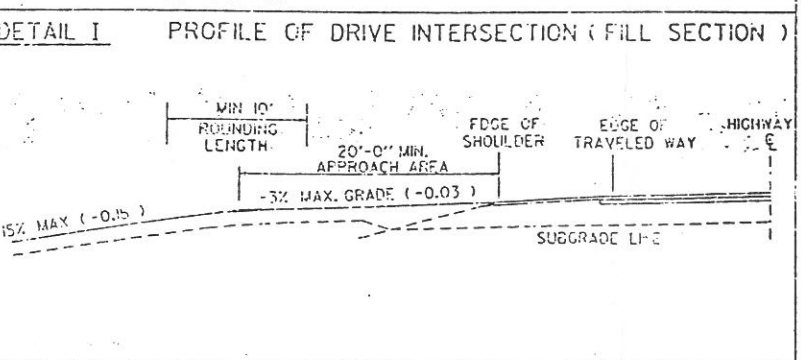
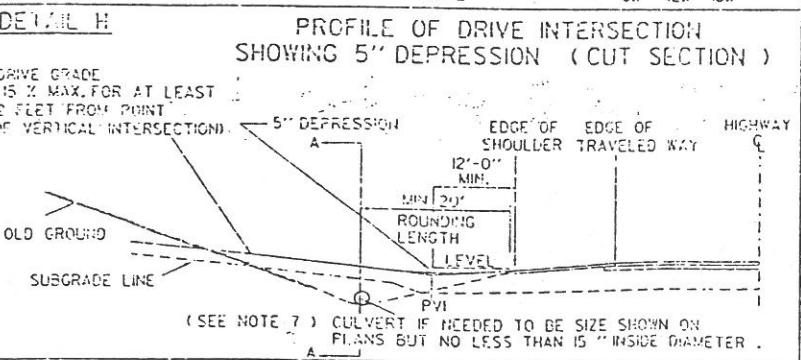
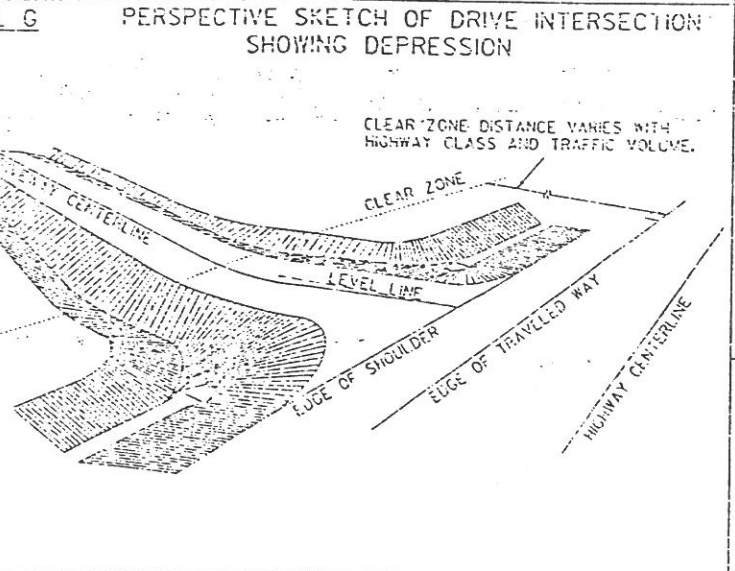
- NOTES:**
- THIS SHEET IS INTENDED FOR USE BY DESIGNERS ON HIGHWAY PROJECTS AND IN CONJUNCTION WITH A PERMIT FOR WORK WITHIN HIGHWAY RIGHT OF WAY (FORM TA 210). ALL CONSTRUCTION REQUIRED BY THE PERMIT AND INDICATED ON THIS SHEET SHALL BE THE RESPONSIBILITY OF THE APPLICANT AND IS SUBJECT TO THE APPROVAL OF THE VT. AGENCY OF TRANSPORTATION. WHEN USED WITH THE PLANS FOR A HIGHWAY CONSTRUCTION PROJECT, THIS SHEET IS INTENDED TO A GUIDE FOR THE DESIGNER CONCERNING MIN/MAX DRIVE WIDTHS AND OTHER VERTICAL AND GEOMETRIC CHARACTERISTICS.
 - ALL COMMERCIAL DRIVES SHALL BE PAVED FROM THE EDGE OF THE TRAVELED WAY TO THE HIGHWAY RIGHT-OF-WAY, TO THE FARTHEST POINT OF CURVATURE ON THE DRIVEWAY EDGE OR AS DIRECTED BY THE DISTRICT TRANSPORTATION ADMINISTRATOR. THIS PA IS INDICATED IN THE DETAILS BY CROSS-HATCHING.
 - DEPTH OF SUB-BASE, BASE, AND PAVEMENT TO BE THE SAME AS HIGHWAY OR MINIMUM AS SHOWN IN DETAIL J WITHIN THE LIMITS OF THE HIGHWAY RIGHT-OF-WAY.
 - VEHICULAR ACCESS FROM PARKING AREAS TO THE RIGHT-OF-WAY AT OTHER THAN APPROVED ACCESS POINTS WILL BE PREVENTED BY THE CONSTRUCTION OF CURBING OR OTHER SUITABLE PHYSICAL BARRIERS.
 - IF CURB IS PRESENT, SEE APPROPRIATE CURB DETAIL STANDARD OR MATCH TOWN/CITY STANDARD CURB TREATMENT.
 - WHERE THE DESIGN HOUR VOLUME OF LEFT TURNING TRAFFIC FROM THE HIGHWAY INTO THE DRIVE IS SUBSTANTIAL, THE VT. AGENCY OF TRANSPORTATION MAY REQUIRE SPECIAL LEFT-TURN TREATMENT AND/OR TRAFFIC SIGNALS. THE AGENCY WILL DETERMINE THE SPECIFIC TREATMENT TO BE USED, AND WILL WORK WITH THE DEVELOPER IN REGARD TO IMPLEMENTATION OF CHANGES ON STATE HIGHWAYS.
 - CIRCULAR DRAINAGE CULVERTS UNDER DRIVES SHALL HAVE A MINIMUM INSIDE DIAMETER (I.D.) OF 15". PIPE ARCHES USED UNDER DRIVES SHALL HAVE A MINIMUM INSIDE CROSS-SECTIONAL AREA EQUIVALENT TO THAT PROVIDED BY A 15" CIRCULAR PIPE.
 - THE OFFSET BETWEEN THE PROPERTY LINE AND THE EDGE OF THE DRIVEWAY MAY BE GOVERNED BY LOCAL ZONING LAWS. DRIVEWAY WIDTH RESTRICTIONS SHOWN PERTAIN ONLY TO THE AREA WITHIN THE HIGHWAY R.O.W..
 - DRIVEWAY GRADES STEEPER THAN THOSE SHOWN MAY BE ALLOWED AS LONG AS A 20' APPROACH AREA IS ACHIEVED FOR THE VEHICLE TO PAUSE BEFORE ENTERING THE HIGHWAY.
 - CORNER SIGHT DISTANCES, EQUAL TO OR GREATER THAN THOSE SHOWN BELOW, SHALL BE PROVIDED IN BOTH DIRECTIONS FOR ALL DRIVES ENTERING ON PUBLIC HIGHWAYS, UNLESS OTHERWISE APPROVED BY THE AGENCY OF TRANSPORTATION. CORNER SIGHT DISTANCE IS MEASURED FROM A POINT ON THE DRIVE AT LEAST 15 FEET FROM THE EDGE OF TRAVELED WAY OF THE ADJACENT ROADWAY AND MEASURED FROM A HEIGHT OF EYE OF 3.5 FEET ON THE DRIVE TO A HEIGHT OF 4.25 FEET ON THE ROADWAY.



TAPERS

LENGTH OF FULL WIDTH STORAGE (D) IS 50' FOR EACH 60 RIGHT TURNING VEHICLES IN DRIVE WITH MINIMUM LENGTH OF 50'	SPEED (MPH)	30	40	50
	**LENGTH (MIN/FTX)	100	140	150
	RATE	8:1	12:1	15:1

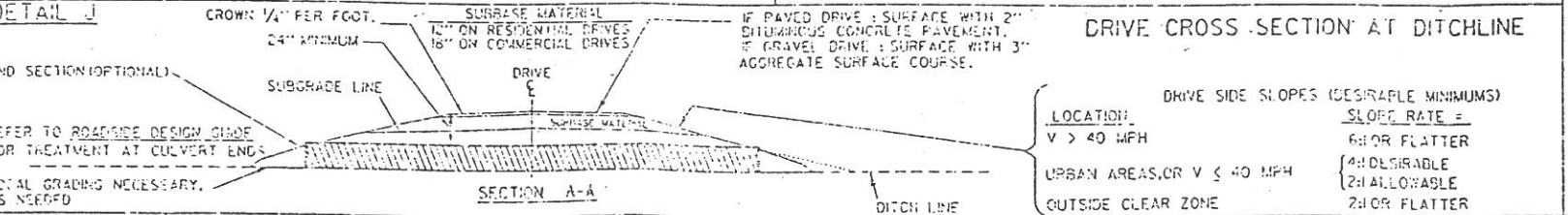
* MINIMUM UNLESS NO OTHER REASONABLE ACCESS IS AVAILABLE AND PRIOR APPROVAL IS GRANTED BY THE A.O.T. OTHER ITEMS SUCH AS TRAFFIC SIGNALS, HIGH TRAFFIC VOLUMES, OR FUNCTIONAL CLASS OF HIGHWAY SHOULD BE CONSIDERED WHEN DETERMINING APPROPRIATE OFFSET DISTANCE.



SIGHT DISTANCE CHART

POSTED SPEED OR DESIGN SPEED (M.P.H.)	MINIMUM STOPPING SIGHT DISTANCE (FT)	MINIMUM CORNER SIGHT DISTANCE ** (1990 AASHTO) (FT)
25	150	275
30	200	355
35	225	445
40	275	560
45	325	660
50	400	770
55	450	925

NOTE: ADVANCE WARNING SIGNS WILL BE REQUIRED IF OBTAINABLE CORNER SIGHT DISTANCES ARE BELOW MINIMUM STOPPING SIGHT DISTANCES.



REVISIONS AND CORRECTIONS
THIS STANDARD SUPERCEDES B-71(7/25/80R), B-71A (3/12/90), AND B-13 (2/14/71).

APPROVED
11 Dec 92
DATE
John D. Tice
DIRECTOR OF ENGINEERING
John D. Tice
DESIGN ENGINEER

STANDARDS FOR RESIDENTIAL AND COMMERCIAL DRIVES
APPROVED FOR THIS PROJECT AND/OR DESIGN IMPLEMENTATION BY FINAL APPROVAL PENDING.
GREATER THAN MINIMUM VALUES TO BE USED WHENEVER FEASIBLE



STANDARD B-71

TOWN OF STAMFORD, VERMONT

Chartered 1753

986 Main Road

Stamford, VT 05352

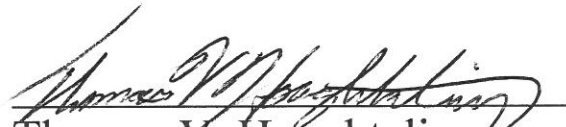
Tel: 802-694-1361


Technical correction to Stamford Town Highway Ordinance section 4.16. Culvert size was in approved draft but was never transferred to electronic and final version.

Selectboard July 8, 2010.


Sheila G. Lawrence


Eunice M. Rice


Thomas V. Houghtaling


LauraLee Smith


Bruce E. Belisle

SB/nlb